

EROSION AND SEDIMENT CONTROL NOTES

CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLIANCE WITH ALL SEDIMENT AND FROSION CONTROL PRACTICES. THE SEDIMENT AND EROSION CONTROL PRACTICES ARE TO BE INSTALLED PRIOR TO ANY MAJOR SOIL DISTURBANCES, AND MAINTAINED UNTIL PERMANENT PROTECTION IS ESTABLISHED.

- TIMELY MAINTENANCE OF SEDIMENT CONTROL STRUCTURES IS THE RESPONSIBILITY OF THE CONTRACTOR. ALL STRUCTURES SHALL BE MAINTAINED IN GOOD WORKING ORDER AT ALL TIMES. THE SEDIMENT LEVEL IN ALL SEDIMENT TRAPS SHALL BE CLOSELY MONITORED AND SEDIMENT REMOVED PROMPTLY WHEN MAXIMUM LEVELS ARE REACHED OR AS ORDERED BY THE ENGINEER. ALL SEDIMENT CONTROL STRUCTURES. SHALL BE INSPECTED WEEKLY PRIOR TO EXPECTED RAIN EVENTS AND AFTER EACH HEAVY RAIN TO ENSURE PROPER OPERATION AS DESIGNED. AN INSPECTION SCHEDULE SHALL BE SET FORTH PRIOR TO THE START OF CONSTRUCTION.
- THE LOCATIONS AND THE INSTALLATION TIMES OF THE SEDIMENT CAPTURING STANDARDS SHALL BE AS ORDERED BY THE ENGINEER, AND IN ACCORDANCE WITH ACCEPTED STANDARDS.
- ALL TOPSOIL NOT TO BE USED FOR FINAL GRADING SHALL BE REMOVED FROM THE SITE IMMEDIATELY AND PLACED IN A STABILIZED STOCKPILE OR FILL AREA. ALL TOPSOIL REQUIRED FOR FINAL GRADING AND STORED ON SITE SHALL BE LIMED FERTILIZED. TEMPORARILY SEEDED AND MULICHED WITHIN 14 DAYS.
- ANY DISTURBED AREAS THAT WILL BE LEFT EXPOSED MORE THAN 21 DAYS AND NOT SUBJECT TO CONSTRUCTION TRAFFIC, SHALL IMMEDIATELY RECEIVE TEMPORARY SEEDING. MULCH SHALL BE USED IF THE SEASON PREVENTS THE ESTABLISHMENT OF A TEMPORARY COVER. DISTURBED AREAS SHALL BE LIMED AND FERTILIZED PRIOR TO
- ALL DISTURBED AREAS WITHIN 500 FEET OF AN INHABITED DWELLING SHALL BE WETTED AS NECESSARY TO PROVIDE DUST CONTROL.
- THE CONTRACTOR SHALL KEEP THE ROADWAYS WITHIN THE PROJECT CLEAR OF SOIL AND DEBRIS AND IS RESPONSIBLE FOR ANY STREET CLEANING NECESSARY DURING THE COURSE OF THE PROJECT.
- SEDIMENT AND EROSION CONTROL STRUCTURES SHALL BE REMOVED AND THE AREA STABILIZED WHEN THE DRAINAGE AREA HAS BEEN PROPERLY STABILIZED BY PERMANENT MEASURES.
- SOIL SEEDING AND FERTILIZER AMENDMENTS SHALL BE PERFORMED IN ACCORDANCE WITH THE CURRENT

EDITION OF "NEW YORK GUIDELINES FOR URBAN EROSION AND SEDIMENT CONTROL".

ENGINEERING, P.E., P.C. FOR APPROVAL

ALL SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH THE CURRENT EDITION OF "NEW YORK GUIDELINES FOR URBAN EROSION AND SEDIMENT CONTROL".

ENGINEERS NOTES TO OWNER / CONTRACTOR

THESE DRAWINGS MAY OR MAY NOT TRULY REFLECT EXISTING CONDITIONS AND THAT SUCH INFORMATION IS INCLUDED ON THE ASSUMPTION THAT IT MAY BE OF INTEREST TO THE CONTRACTOR, BUT THE ENGINEER, OWNER AND THEIR CONSULTANTS DO NOT ASSUME RESPONSIBILITY FOR ITS ACCURACY OR COMPLETENESS. THE CONTRACTOR HAS AN OBLIGATION TO DETERMINE FOR ITESELF THE TRUE NATURE OF EXISTING CONDITIONS.

- A PRE-CONSTRUCTION SITE INSPECTION WITH CRONIN ENGINEERING, P.E., P.C. IS REQUIRED W/ THE OWNER AND CONTRACTOR PRESENT TO CONFIRM THE CONSTRUCTION PROCEDURE.
- THERE SHALL BE NO MODIFICATION TO ANY ASPECT OF THIS PLAN WITHOUT FIRST CONTACTING CRONIN
- PRIOR TO ANY EXCAVATION, THE OWNER AND/OR CONTRACTOR SHALL CALL THE UNDERGROUND LINE LOCATION SERVICE (CODE 53) AT (800)-962-7962. FOR MORE INFORMATION, VISIT WWW.DIGSAFELYNEWYORK.COM.
- EROSION & SEDIMENT CONTROL MEASURES AS SHOWN IN THIS PLAN SET SHALL BE IN PLACE PRIOR TO THE COMMENCEMENT OF ANY SITE WORK. IF UNFORESEEN FIELD CONDITIONS ARE ENCOUNTERED WHICH PROHIBIT THE INSTALLATION OF CERTAIN EROSION & SEDIMENT CONTROL MEASURES AS SHOWN, IT SHALL BE THE OWNER AND/OR CONTRACTOR'S RESPONSIBILITY TO CONTACT CRONIN ENGINEERING, P.E., P.C. IMMEDIATELY TO DISCUSS ALTERNATIVE METHODS. IT SHALL BE THE OWNER AND/OR CONTRACTOR'S RESPONSIBILITY TO ENSURE THE INTEGRITY OF ALL EROSION & SEDIMENT CONTROL MEASURES AT ALL TIMES THROUGHOUT THE DURATION OF THE
- CRONIN ENGINEERING, P.E., P.C. MAKES NO REPRESENTATIONS OR CERTIFICATIONS AS TO THE INTEGRITY, LOCATION OR EXISTENCE OF SUBSURFACE STRUCTURES OR SOIL CONDITIONS WITH RESPECT TO STABILITY AND SUITABILITY FOR THE INTENDED PURPOSE. IT IS THE RESPONSIBILITY OF THE OWNER AND/OR CONTRACTOR TO VERIFY ALL SUBSURFACE CONDITIONS AND ENSURE THAT ALL IMPROVEMENTS ARE PLACED ON MATERIAL WITH A SUITABLE BEARING CAPACITY.
- CRONIN ENGINEERING, P.E., P.C. MAKES NO REPRESENTATION OR CERTIFICATIONS AS TO THE QUANTITY OF MATERIAL NEEDED OR TO BE REMOVED FOR THE SUCCESSFUL CONSTRUCTION OF THE PROJECT. IT IS THE RESPONSIBILITY OF THE OWNER AND/OR CONTRACTOR TO VERIFY THE QUANTITY OF MATERIAL NEEDED OR TO BE REMOVED TO SUCCESSFULLY CONSTRUCT THE PROJECT.
- IN THE EVENT THAT FIELD CONDITIONS ARE DIFFERENT THAN WHAT IS PRESENTED IN THIS PLAN SET, IT IS THE RESPONSIBILITY OF THE OWNER AND/OR CONTRACTOR TO NOTIFY CRONIN ENGINEERING, P.E., P.C. PRIOR TO CONTINUING WITH ANY FURTHER SITE WORK.
- IF UNFORESEEN SUBSURFACE CONDITIONS ARE ENCOUNTERED (I.E. ROCK, GROUNDWATER, ETC.), THE OWNER AND/OR CONTRACTOR SHALL STOP WORK AND NOTIFY CRONIN ENGINEERING, P.E., P.C. ALL NECESSARY MODIFICATIONS OR CHANGES SHALL BE DISCUSSED WITH AND APPROVED BY CRONIN ENGINEERING. P.E., P. PRIOR TO CONTINUING WITH ANY FURTHER SITE WORK. FURTHERMORE, THE OWNER AND/OR CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS IF BLASTING IS REQUIRED.
- IT IS THE OWNER AND/OR CONTRACTOR'S SOLE RESPONSIBILITY TO FOLLOW OSHA, NYS AND ANY OTHER APPLICABLE CODES OR REQUIREMENTS THROUGHOUT THE DURATION OF THE PROJECT. THIS INCLUDES BUT IS NOT LIMITED TO PROVIDING ADEQUATE BRACING AND GUARANTEEING THE STABILITY OF EXCAVATIONS AND OTHER
- CRONIN ENGINEERING, P.E. P.C. MAKES NO REPRESENTATION AS TO THE QUALITY (I.E. CONTAMINATION), IF ANY, OF THE SOILS ON THIS SITE. THE OWNER AND/OR CONTRACTOR ARE RESPONSIBLE TO CONDUCT ANY AND ALL

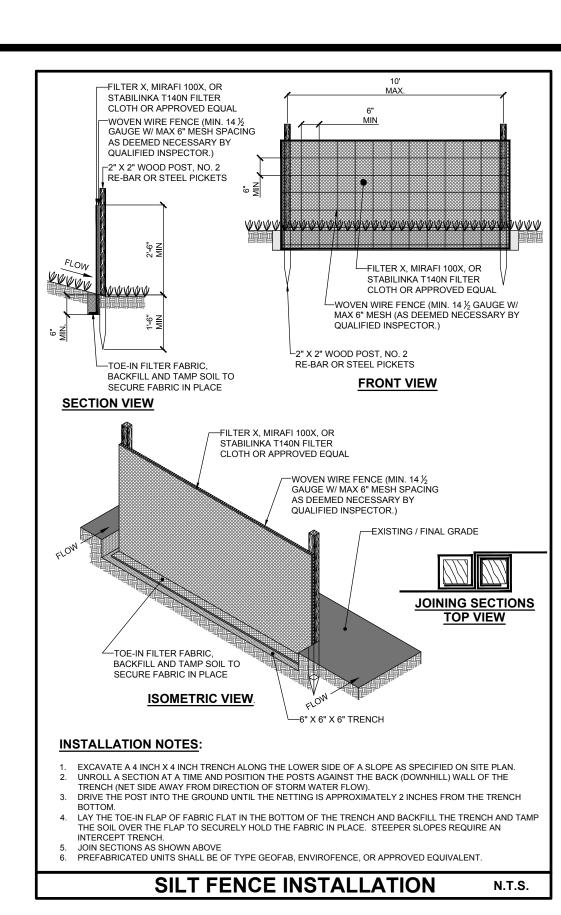
TESTING AS MAY BE REQUIRED TO ENSURE THE SITE HAS NO CONTAMINATED SOILS.

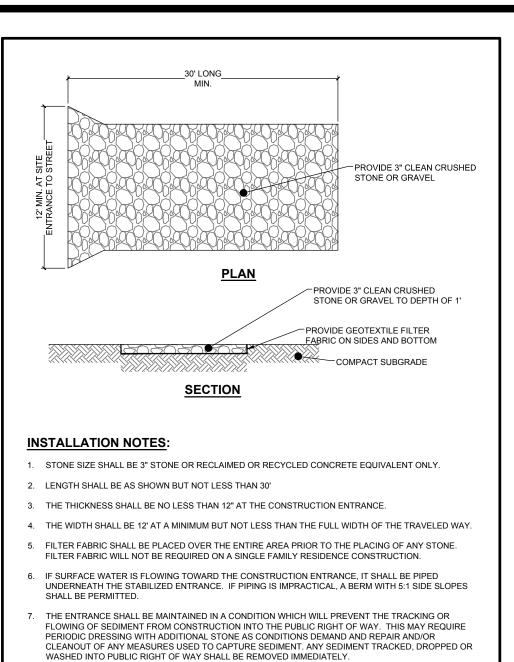
- CRONIN ENGINEERING, P.E. P.C. MAKES NO REPRESENTATION AS TO THE SOIL BEARING CAPACITY OF THE SOILS ON SITE. THE OWNER AND/OR ARCHITECT ARE RESPONSIBLE TO CONDUCT ANY AND ALL TESTING AS MAY BE REQUIRED TO ENSURE THE CONSTRUCTION OF THE FOUNDATION MEETS THE REQUIREMENTS OF THE NEW YORK STATE BUILDING CODE AND THE BUILDING CODE OF THE VILLAGE OF BUCHANAN. FOUNDATION DESIGN IS NOT PROVIDED BY THIS OFFICE AND IS THE RESPONSIBILITY OF THE OWNER/ARCHITECT/BUILDER.
- THE OWNER/BUILDER/ARCHITECT ARE RESPONSIBLE TO ENSURE THAT THE BUILDING FOUNDATION MEETS ALL VILLAGE CODES AND THE NEW YORK STATE RESIDENTIAL BUILDING CODE. IF UNSUITABLE MATERIAL IS ENCOUNTERED, THE OWNER SHALL MAKE PROVISIONS (I.E. DEEPER FOOTING, SPREAD FOOTING, GRADE BEAMS, PILES, ETC.) AS NECESSARY TO ENSURE A CODE COMPLIANT FOUNDATION TO THE SATISFACTION OF THE VILLAGE OF BUCHANAN BUILDING DEPARTMENT.

CONSTRUCTION SEQUENCE

THE PROPOSED PROJECT CONSISTS OF THE REPLACEMENT OF AN EXISTING COMMERCIAL BUILDING AND MODIFICATIONS O THE EXISTING SITE LAYOUT ON A 0.37 ACRE PARCEL IN THE COMMERCIAL (C-2) OVERLAY ZONING DISTRICT.

- THE FOLLOWING IS THE ANTICIPATED SEQUENCE OF CONSTRUCTION
- OBTAIN THE NECESSARY APPROVAL SIGNATURES ON THE SITE DEVELOPMENT PLANS FROM THE VILLAGE OF
- CONDUCT A PRE-CONSTRUCTION MEETING AT THE SITE WITH REPRESENTATIVES FROM THE VILLAGE, THE SITE GENERAL CONTRACTOR AND THE OWNER.
- FILE THE NECESSARY DOCUMENTS TO OBTAIN THE REQUIRED BUILDING PERMIT(S).
- INSTALL THE NECESSARY SOIL EROSION AND SEDIMENT CONTROL PRACTICES.
- PERFORM ANY REMOVALS AS REQUIRED PER THE SITE PLAN.
- CONSTRUCT THE FOOTINGS AND FOUNDATIONS FOR THE PROPOSED BUILDING.
- PROCEED WITH THE CONSTRUCTION OF THE BUILDING.
- INSTALL NEW WATER SERVICE, SEWER SERVICE, CULTECS, AND OIL WATER SEPARATOR.
- CONSTRUCT THE PROPOSED SIDEWALKS AND CURBS PER THE SITE PLAN.
-). PERFORM FINAL GRADING AROUND THE BUILDING.
- FINALIZE BUILDING CONSTRUCTION.
- THROUGHOUT THE PROCESS, SCHEDULE INSPECTIONS AS NECESSARY WITH THE VILLAGE OF BUCHANAN BUILDING DEPARTMENT
- 3. INSTALL LANDSCAPE PLANTINGS.
- 4. PROVIDE PARKING MARKINGS PER APPROVED SITE PLAN.
- . FINALIZE AND STABILIZE ALL SITE CONSTRUCTION. REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL
- APPLY FOR AND OBTAIN CERTIFICATE OF OCCUPANCY.





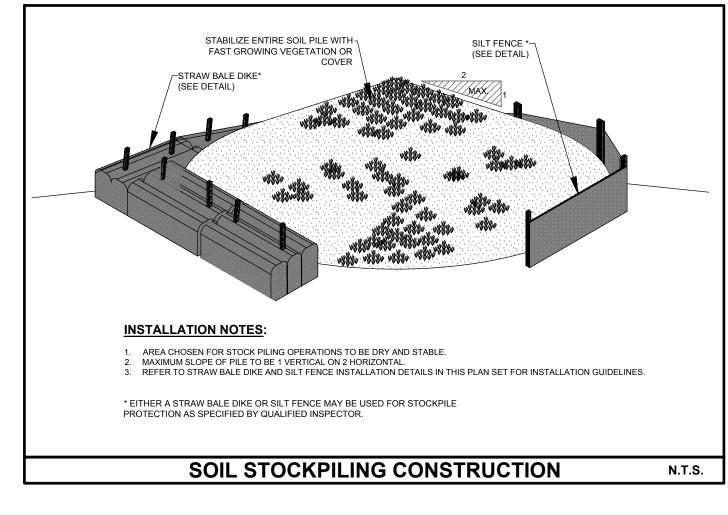
WHEELS OF CONSTRUCTION VEHICLES SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO

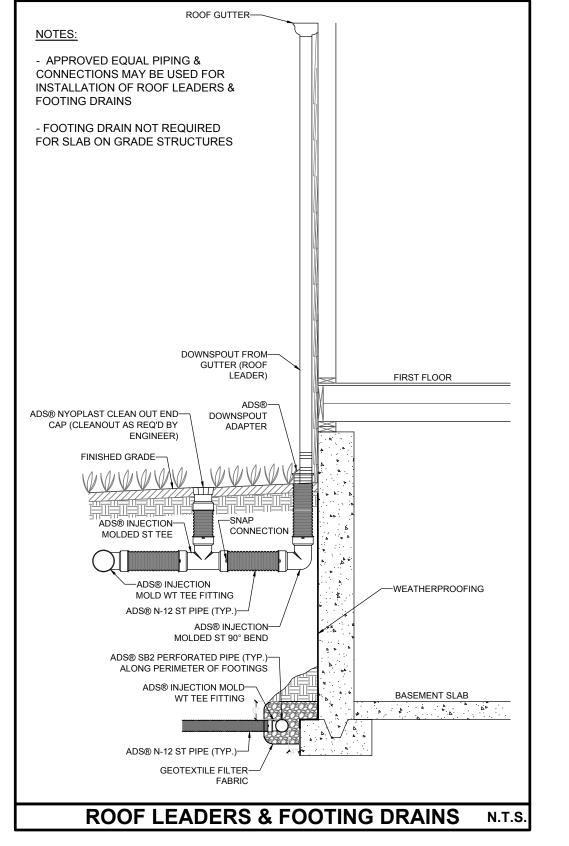
PERIODIC INPECTION AND MAINTENANCE SHALL BE PROVIDED FOR THE DURATION OF

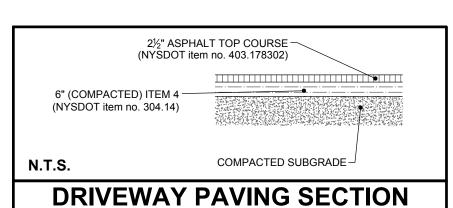
CONSTRUCTION OR UNTIL FINAL GRADE IS ACHIEVED.

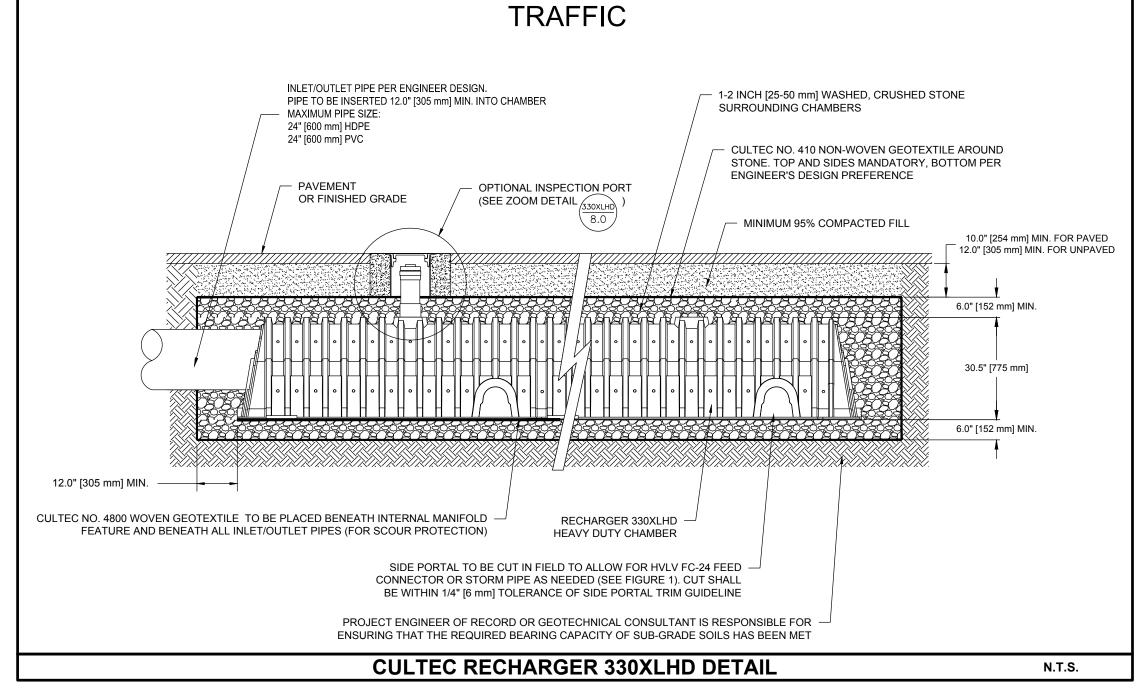
ENTERING ONTO THE PUBLIC RIGHT OF WAY. WHEN WASHING IS REQUIRED. IT SHALL BE DONE IN AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROPRIATE SEDIMENT TRAPPING

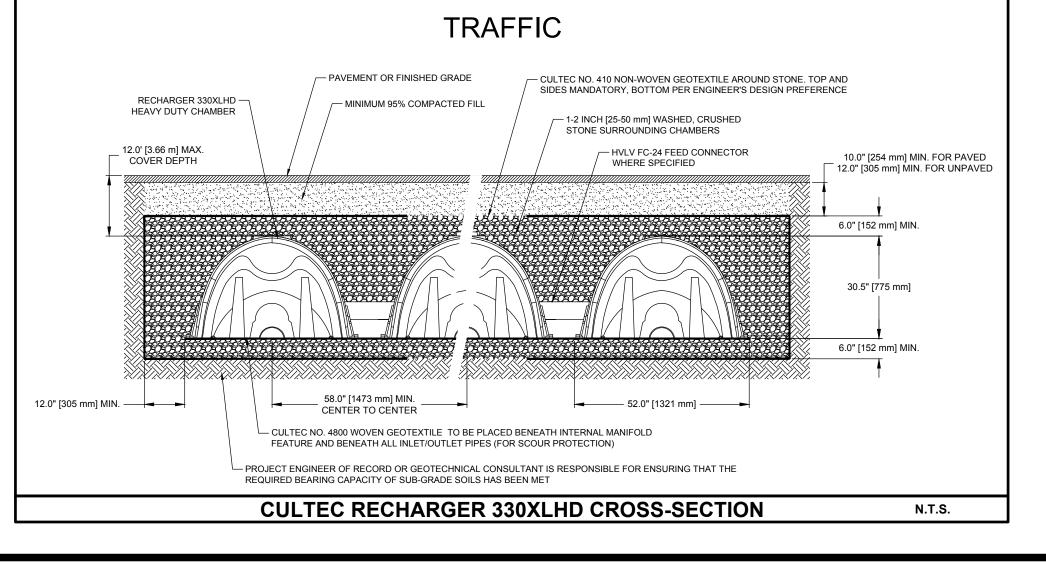
STABILIZED CONSTRUCTION ENTRANCE

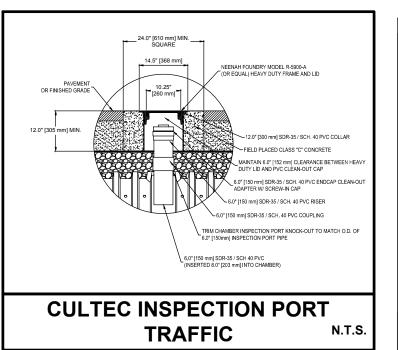


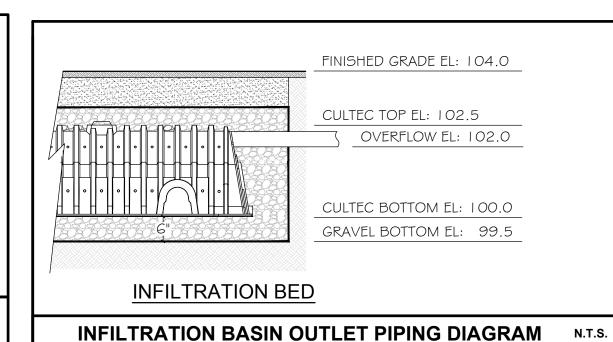


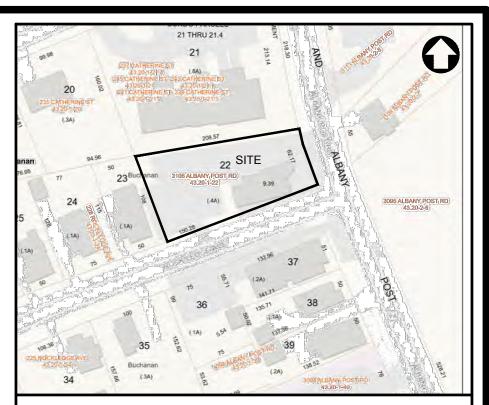












LOCATION MAP



SCALE: 1" = 150'

OWNER/APPLICANT

GALLON MEASURE 3106 ALBANY POST ROAD BUCHANAN, NY 10511

• UNDER NEW YORK STATE EDUCATIONAL LAW ARTICLE 145 SECTION 7209 (2) IT UNLAWFUL FOR ANY PERSON TO ALTER ANY ITEM ON THIS DRAWING. UNLESS ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER. IF ANY ITEM IS ALTERED. THE ALTERING ENGINEER SHALL AFFIX TO THE ITEM HIS SEAL AND THE NOTATION "ALTERED BY" FOLLOWED BY HIS SIGNATURE AND THE DATE OF SUCH ALTERATION AND A SPECIFIC DESCRIPTION OF THE ALTERATION • COPYRIGHT "2025" BY CRONIN ENGINEERING, P.E., P.C. ALL RIGHTS RESERVED.

REVISIONS		
7		
6		
5		
4		
3		
2	ENGINEER COMMENTS DATED 11-19-2025	DECEMBER 09, 2025
1	ENGINEER COMMENTS DATED 10-16-2025	NOVEMBER 10, 2025
#	REASON	DATE

MUNICIPAL TAX IDENTIFICATION: BLOCK: SUBLOT: DRAWN BY: AD / KW CHECKED: JA

PROJECT: ALBANY POST RD

251004

IOB #:

OCTOBER 9, 2025

PATRICK M. BELL, PE

LICENSE #087679

(914) 736-3664

39 Arlo Lane Cortlandt Manor, New York 10567

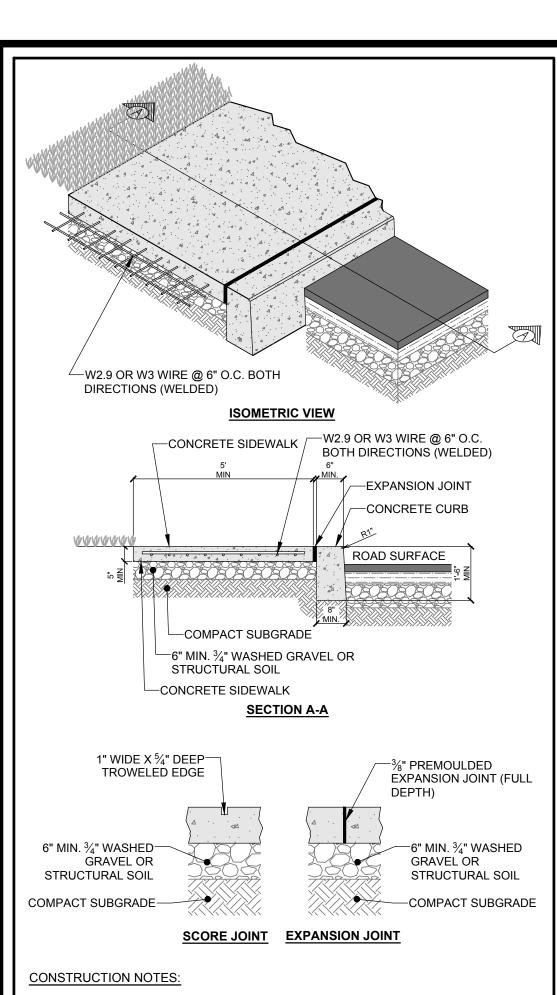
CONSTRUCTION DETAILS

SITE DEVELOPMENT PLAN

GALLON MEASURE

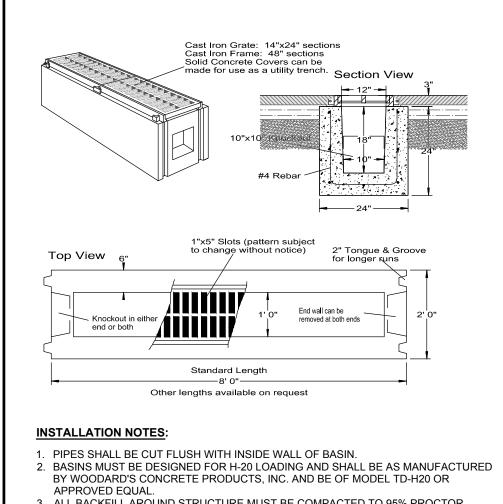
LOCATION: 3106 ALBANY POST ROAD VILLAGE OF BUCHANAN (TOWN OF CORTLANDT), NY

CD-3.1 SHEET 5 OF 6



- CONTRACTOR SHALL COMPLY WITH REQUIREMENTS OF SECTION 501 NYSDOT STANDARD SPECIFICATIONS CODE FOR TRANSPORTING, AND PLACING OF PORTLAND CEMENT CONCRETE.
- CONCRETE TO BE PLACED IN ONE COURSE TO THE FULL DEPTH.
- PROVIDE CONCRETE CLASS "A" AS PER NYSDOT SECTION 501-2.
- CURING OF CONCRETE SHALL CONFORM SECTION 502 NYS CODE.
- SIDEWALK TO BE PLACED ON 6" THICK BASE OF COMPACTED ¾"GRAVEL OR STRUCTURAL SOIL (AS
- SIDEWALK SLAB SHALL HAVE A MINIMUM THICKNESS OF 5". AND 7" MIN. AT DRIVEWAY SECTIONS. WIRE FABRIC TO BE W2.9 OR W3 WIRE AT 6" CENTERS TRANSVERSELYAND LONGITUDINALLY, AND
- SHALL BE EMBEDDED TO MID-DEPTH IN THE SLAB.
- TRANSVERSE EXPANSION JOINTS SHALL BE PLACED TO THE FULL DEPTH OF SLAB AND BE SPACED 20 FEET APART. THE EDGES OF TRANSVERSE JOINTS TO BE FINISHED WITH AN EDGING TOOL HAVING A
- ALL JOINTS SHALL BE PREMOLDED BITUMINOUS TYPE, 3/8" OR 1/2". CONCRETE SURFACE TO BE SCORED AND TOOLED AT INTERVALSOF 5 FEET.
- THE CONCRETE SHALL BE FINISHED TO PRODUCE A SMOOTHSURFACE, THEN LIGHTLY BROOMED TO A UNIFORM TEXTURE, AND SHOULD BE SLIP RESISTANT.
- CONTRACTOR TO PREVENT AGAINST ANY LOW SPOTS WHERE WATER CAN COLLECT AND ANY POSSIBLE REDIRECTION OF STORMWATER
- CONTRACTOR TO USE RESILIENT JOINT FILLER ACCORDING TO SECTION 705-07 NYS CODE AND INSTALL AT ALL JOINTS BETWEEN SIDEWALK, RAMPS, AND CURBS.
- CONCRETE SIDEWALK TO RECEIVE (2) TWO COATS OF ANTI-SPALLING COMPOUND.

CONCRETE SIDEWALK INSTALLATION N.T.S.



- 3. ALL BACKFILL AROUND STRUCTURE MUST BE COMPACTED TO 95% PROCTOR DENSITY TO AVOID SETTLEMENT OF MATERIAL AROUND THE STRUCTURE.
- BASINS TO BE CHECKED PERIODICALLY DURING CONSTRUCTION BY A QUALIFIED INSPECTOR AND CLEANED OUT AS NECESSARY DURING CONSTRUCTION.

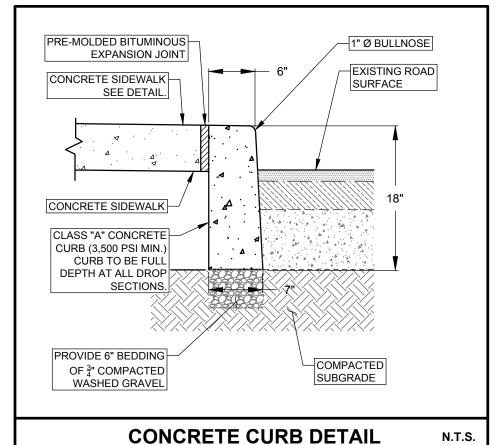
TRENCH DRAIN INSTALLATION

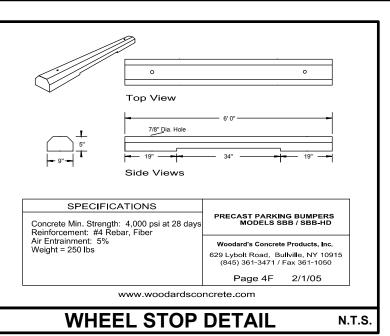
TIE INTO EXISTING GROUND SEE PAVEMENT RECONSTRUCTION OPTIONS A AND B ON THIS SHEE TYPICAL SIDEWALK CROSS SECTION CURBED WITHOUT BUFFER ZONE FULL DEPTH SAWCUT PAVEMENT RECONSTRUCTION OPTION A: HOT MIX ASPHALT SEE NOTE 18

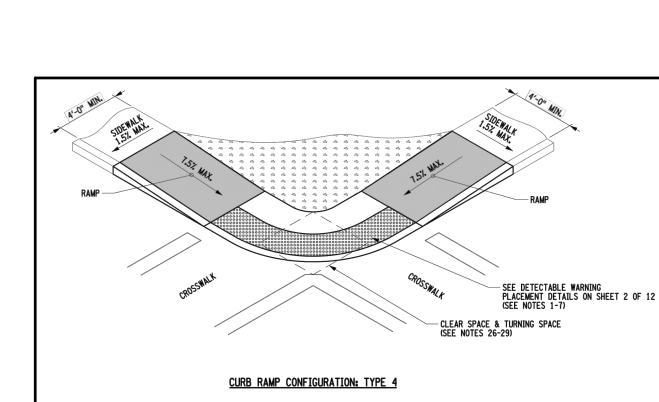
PED. ACCESS ROUTE/ SIDEWALK WIDTH SLOPE TRANSITION

CURB (TYPE VARIES)
(SEE NYSDOT 609 SERIES STANDARD SHEETS)

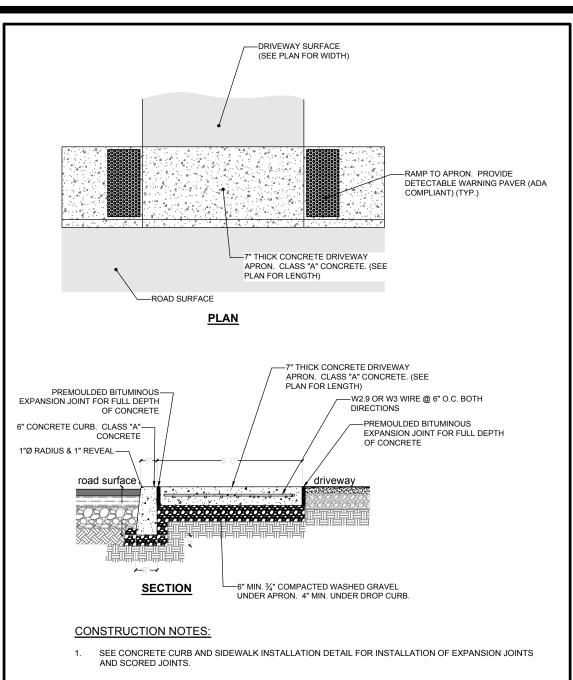
NYSDOT SIDEWALK & CURB DETAIL



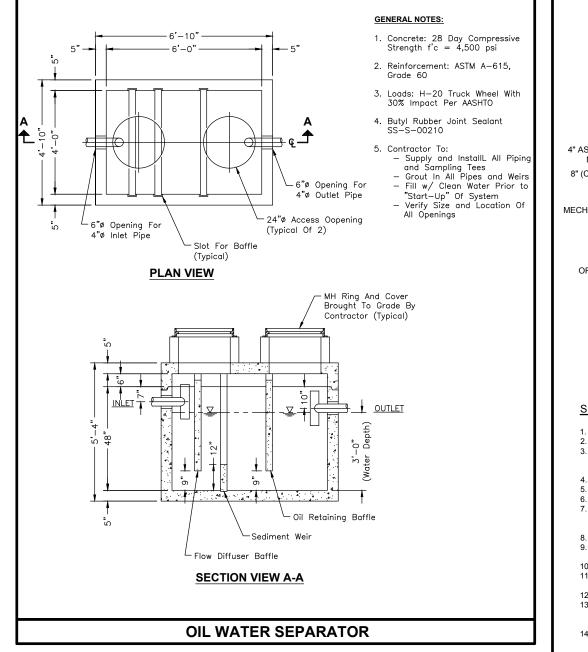


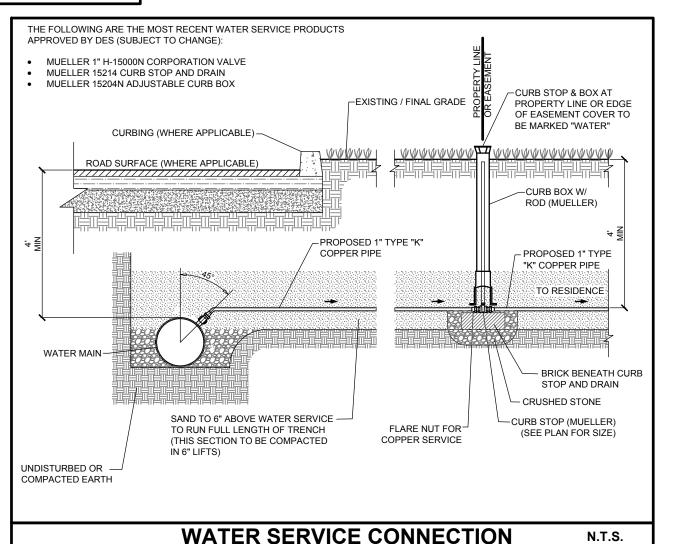


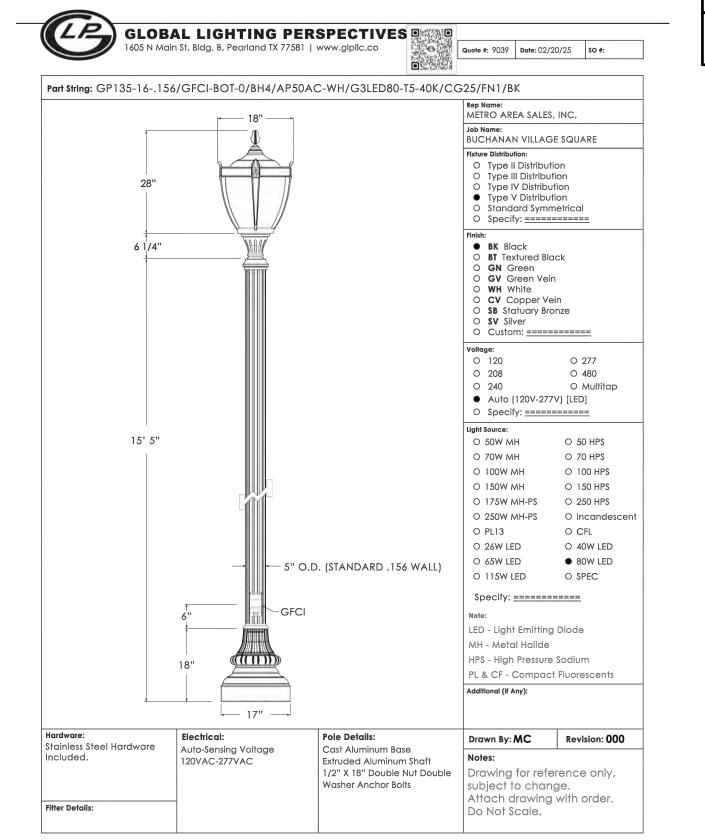
NYSDOT CURB RAMP DETAIL

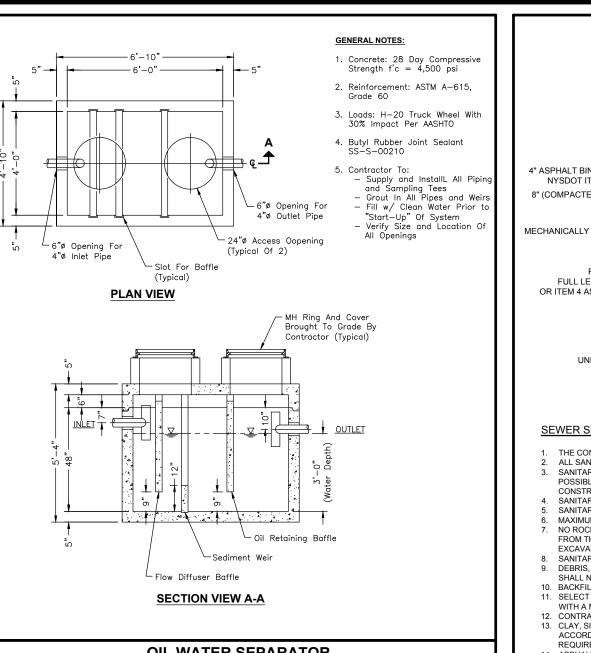


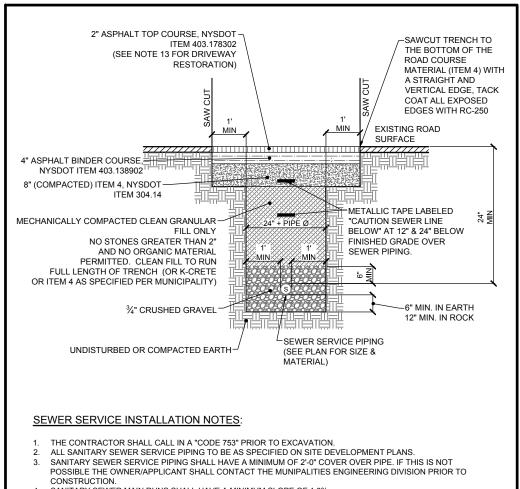
CONCRETE DROP CURB & DRIVEWAY APRON N.T.S









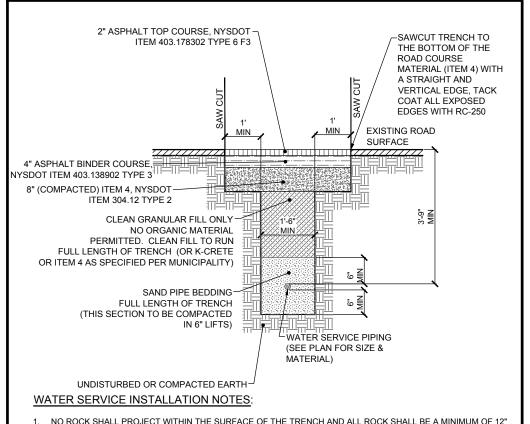


- SANITARY SEWER MAIN RUNS SHALL HAVE A MINIMUM SLOPE OF 1.0%.
 SANITARY SEWER SERVICE RUNS SHALL HAVE A MINIMUM SLOPE OF 2.0% FOR 4" DIA. PIPES.
- MAXIMUM RUN BETWEEN MANHOLES SHALL BE 300'. NO ROCK SHALL PROJECT WITHIN THE SURFACE OF THE TRENCH AND ALL ROCK SHALL BE A MINIMUM OF 12" FROM THE PIPE HORIZONTALLY. IF BEDROCK IS ENCOUNTERED DURING INSTALLATION, IT SHALL BE EXCAVATED TO A MINIMUM DEPTH OF 4' + PIPE Ø + 6" AND REPLACED WITH 6" OF SAND. SANITARY SEWER SHALL BE ANCHORED AT EACH BELL WHERE SLOPE OF PIPE EXCEEDS 20%.
- DEBRIS, FROZEN MATERIAL, LARGE CLODS OR STONES, ORGANIC MATTER OR OTHER UNSUITABLE MATERIALS SHALL NOT BE USED AS BACKFILL.

 BACKFILL SHALL BE PLACED SO AS NOT TO DISTURB THE PIPE ALIGNMENT.
- SELECT AND BACKFILL MATERIAL SHALL BE PLACED IN 12" LIFTS (PRIOR TO COMPACTION) AND COMPACTED WITH A MECHANICAL TAMPER.
- 2. CONTRACTOR TO NOTIFY MUNICIPALITY 48 HOURS IN ADVANCE FOR TRENCH INSPECTION.
 8. CLAY, SILT LOAM OR NON SOILS REQUIRE SHORING AND BRACING. SHEETING AND BRACING SHALL BE USED IN ACCORDANCE WITH OSHA AND THE NYS DEPARTMENT OF LABOR INDUSTRIAL CODE. RULE 753 WHERE
- REQUIRED. SHEETING & BRACING ARE REQUIRED WHERE TRENCH DEPTHS EXCEED 5' IN HEIGHT.

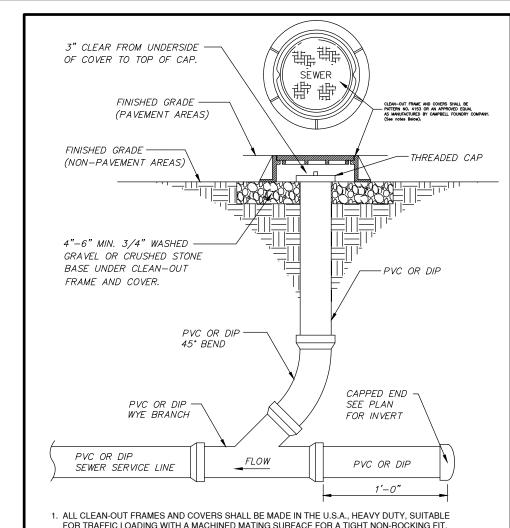
 14. ASPHALT DRIVEWAY RESTORATION TO CONSIST OF 6" COMPACTED ITEM 4 (NYSDOT ITEM 304.05) UNDER 2.5" OF ASPHALT CONCRETE TOP COURSE (NYSDOT ITEM 403.178302)

SEWER SERVICE INSTALLATION W/ PAVEMENT RESTORATION



- NO ROCK SHALL PROJECT WITHIN THE SURFACE OF THE TRENCH AND ALL ROCK SHALL BE A MINIMUM OF 12' FROM THE PIPE HORIZONTALLY. IF BEDROCK IS ENCOUNTERED DURING INSTALLATION, IT SHALL BE EXCAVATED TO A MINIMUM DEPTH OF 4' + PIPE Ø + 6" AND BEDDING AND TRENCH BACKFILL MATERIAL SHALL
- CLAY, SILT LOAM, OR NON-HOMOGENOUS SOILS REQUIRE SHORING AND BRACING. SHEETING AND BRACING SHALL BE USED IN ACCORDANCE WITH OSHA AND THE NYS DEPARTMENT OF LABOR INDUSTRIAL CODE, RULE 753 WHERE REQUIRED. SHEETING & BRACING ARE REQUIRED WHERE TRENCH
- ANY UNSUITABLE MATERIAL EXCAVATED FROM THE TRENCH SHALL NOT USED AS BACKFILL AND SHALL BE REPLACED BY SUITABLE MATERIAL APPROVED BY THE DESIGN ENGINEER.

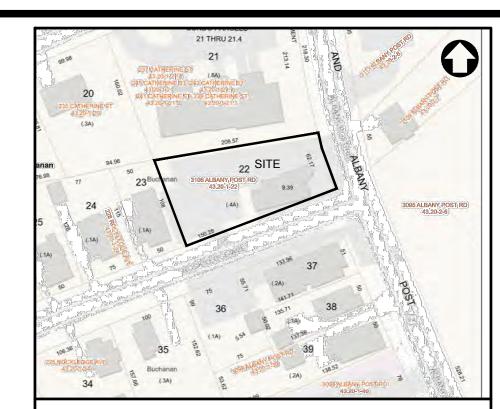
WATER SERVICE INSTALLATION W/ PAVEMENT RESTORATION



FOR TRAFFIC LOADING WITH A MACHINED MATING SURFACE FOR A TIGHT NON-ROCKING FIT. 2. CLEAN-OUT COVERS SHALL CONTAIN THE WORD "SEWER" CAST IN RAISED LETTERS 3. THE CONTRACTOR SHALL BE RESPONSIBLE TO KEEP THE COVER PATTERN CLEAN AND FREE OF EXCESS PAVEMENT, MORTAR, AND DIRT THAT MAY HINDER REMOVAL OR OBSCURE THE LETTERING UNTIL THE MUNICIPALITY HAS ACCEPTED THE SEWER.

GRAVITY SEWER SERVICE CLEANOUT

N.T.S



LOCATION MAP

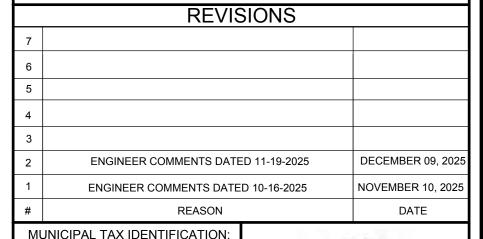


SCALE: 1" = 150'

OWNER/APPLICANT

GALLON MEASURE 3106 ALBANY POST ROAD BUCHANAN, NY 10511

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BLOCK: SUBLOT: DRAWN BY: AD / KW CHECKED: JA ROJECT: ALBANY POST RD OCTOBER 9, 2025

251004

PATRICK M. BELL, PE LICENSE #087679



39 Arlo Lane Cortlandt Manor, New York 10567

CONSTRUCTION DETAILS

SITE DEVELOPMENT PLAN GALLON MEASURE

LOCATION: 3106 ALBANY POST ROAD VILLAGE OF BUCHANAN (TOWN OF CORTLANDT), NY

CD-3.2 SHEET 6 OF 6